

Fulfilling the Future

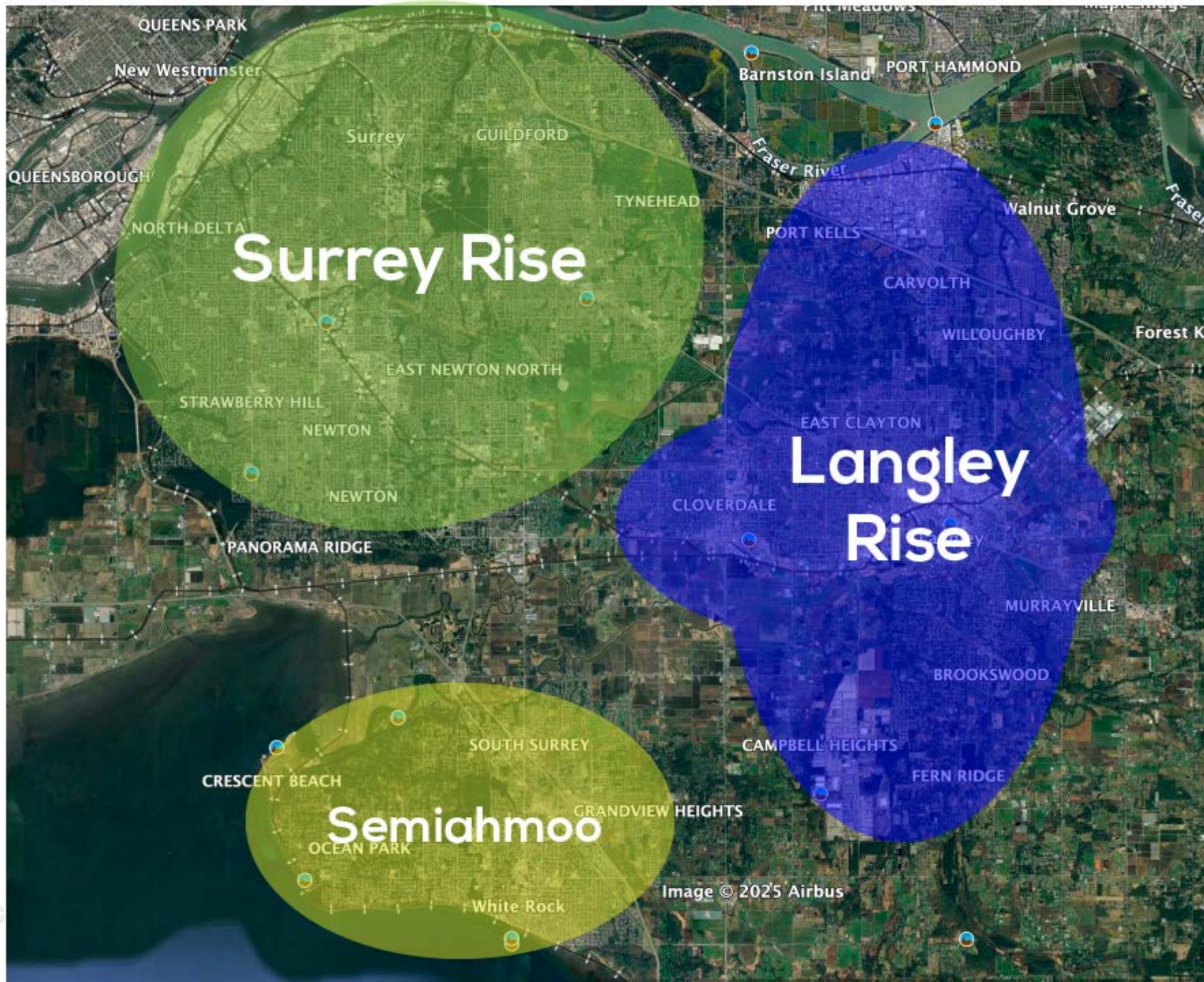
The Transit the South Fraser Needs to Start Planning For



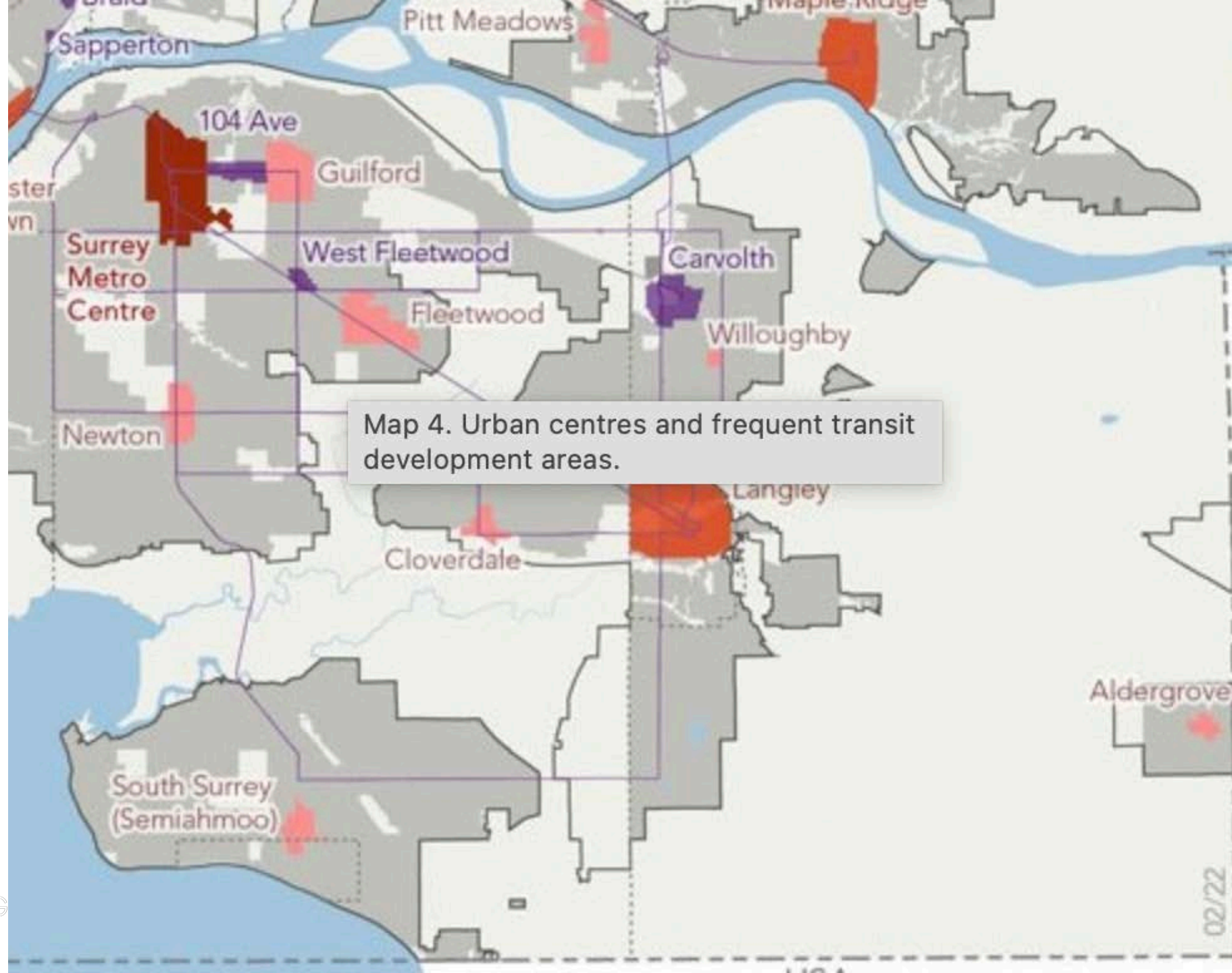
FOR A GREATER REGION



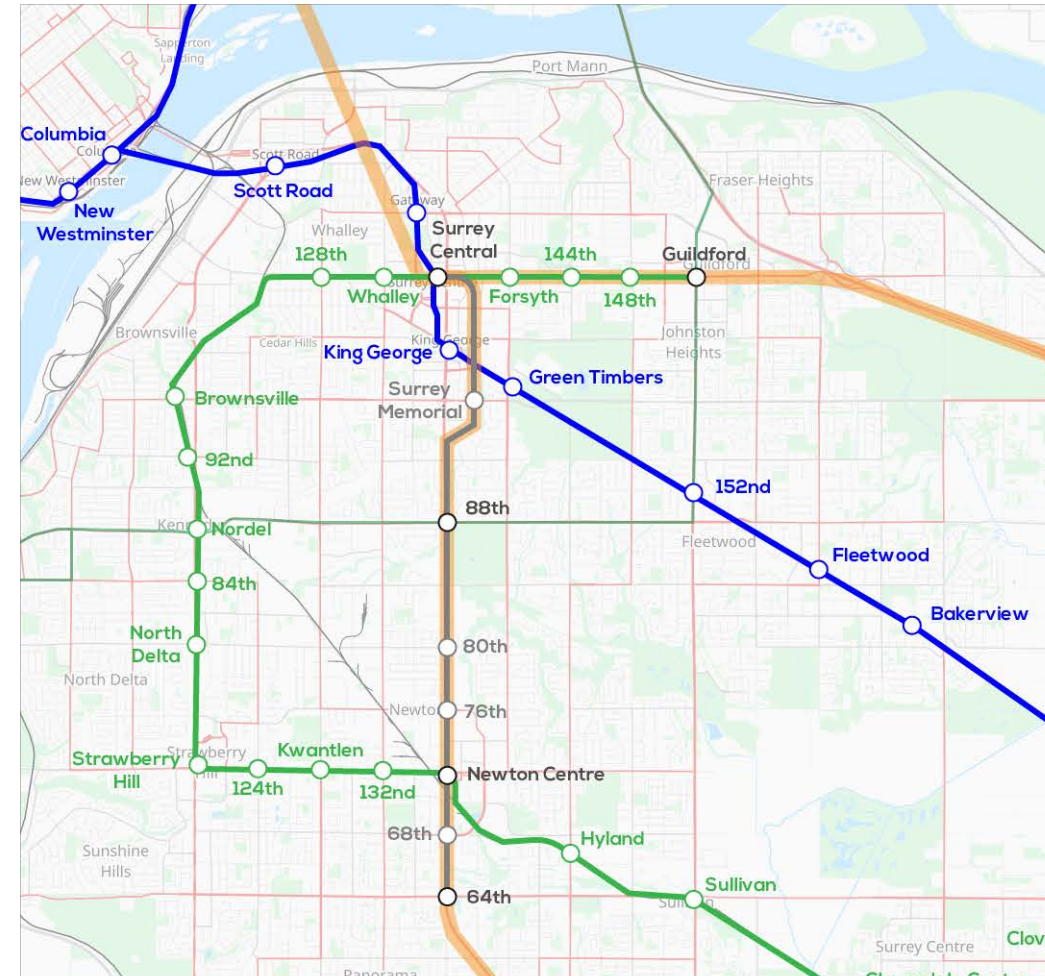
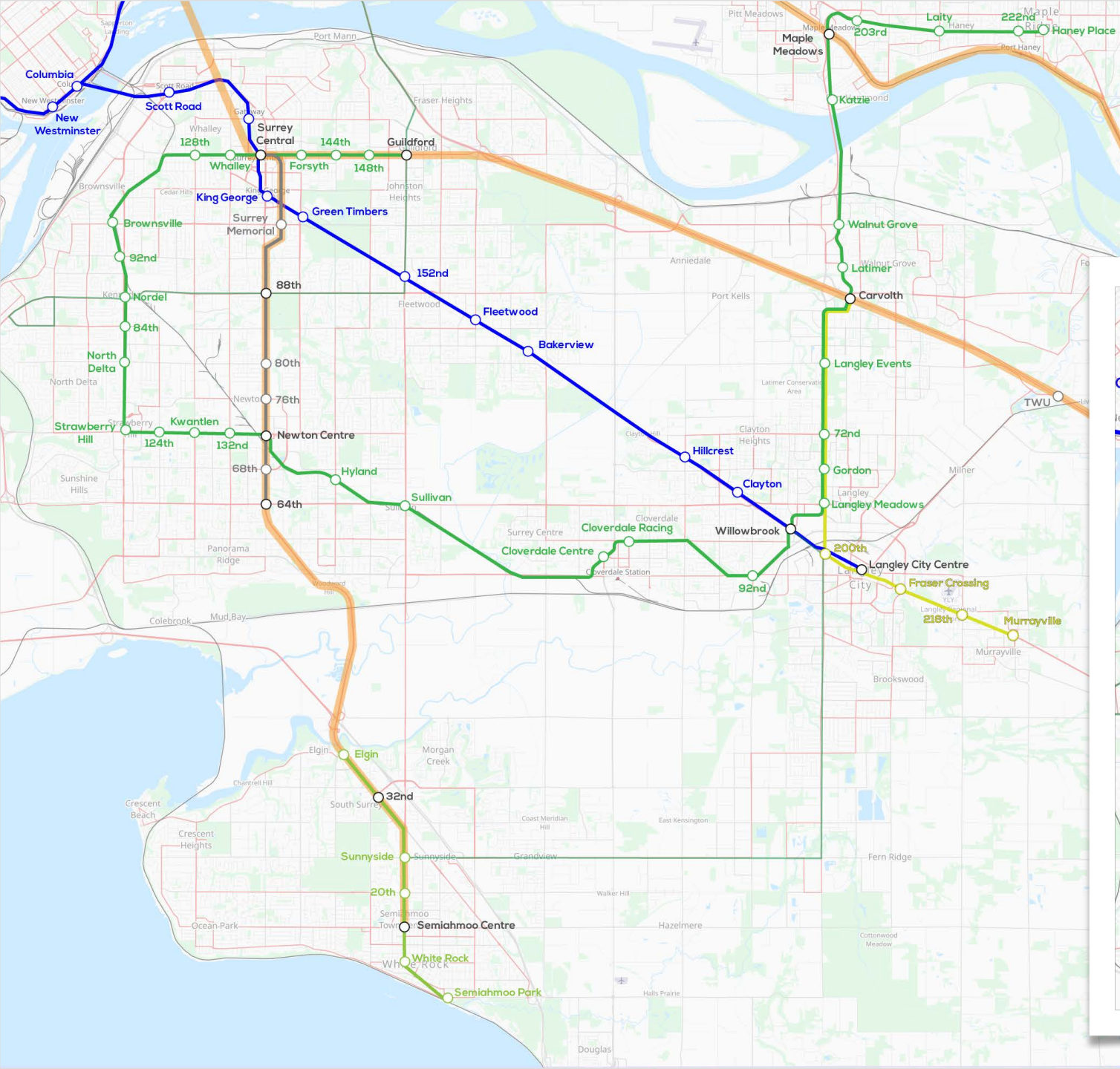
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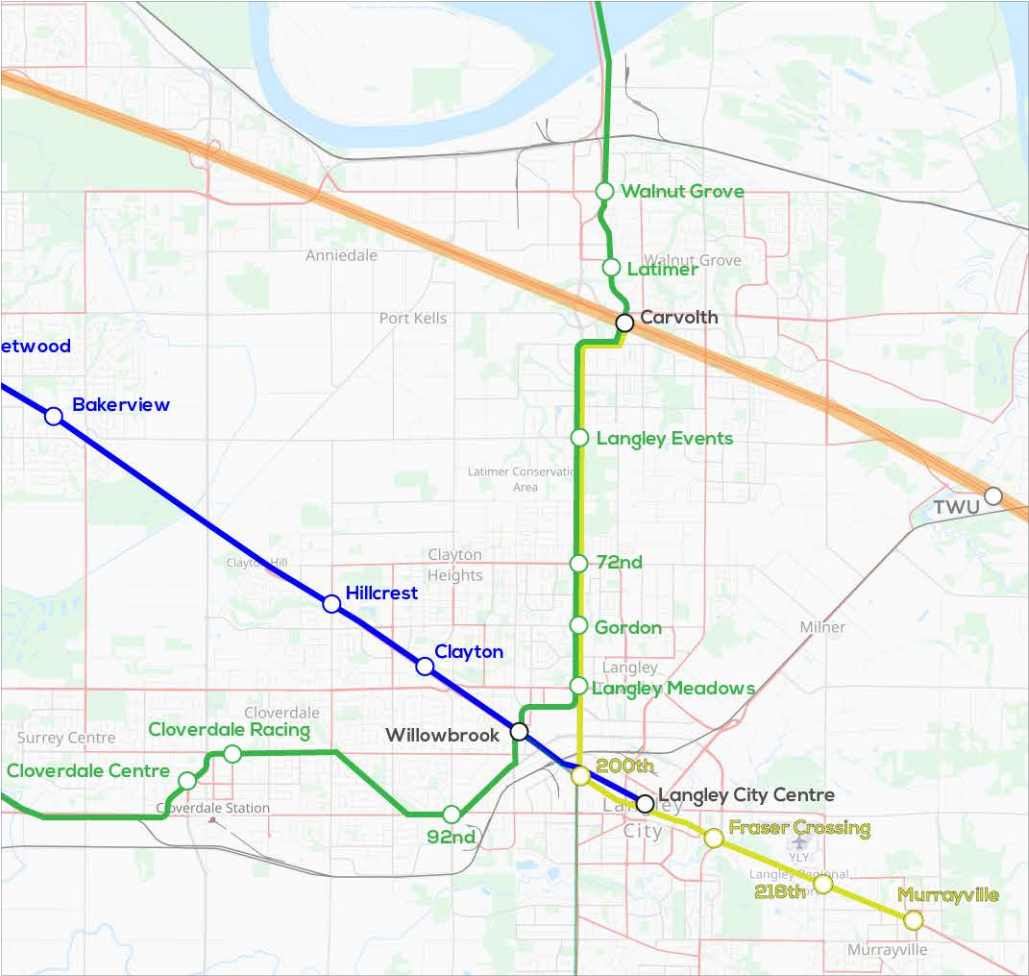
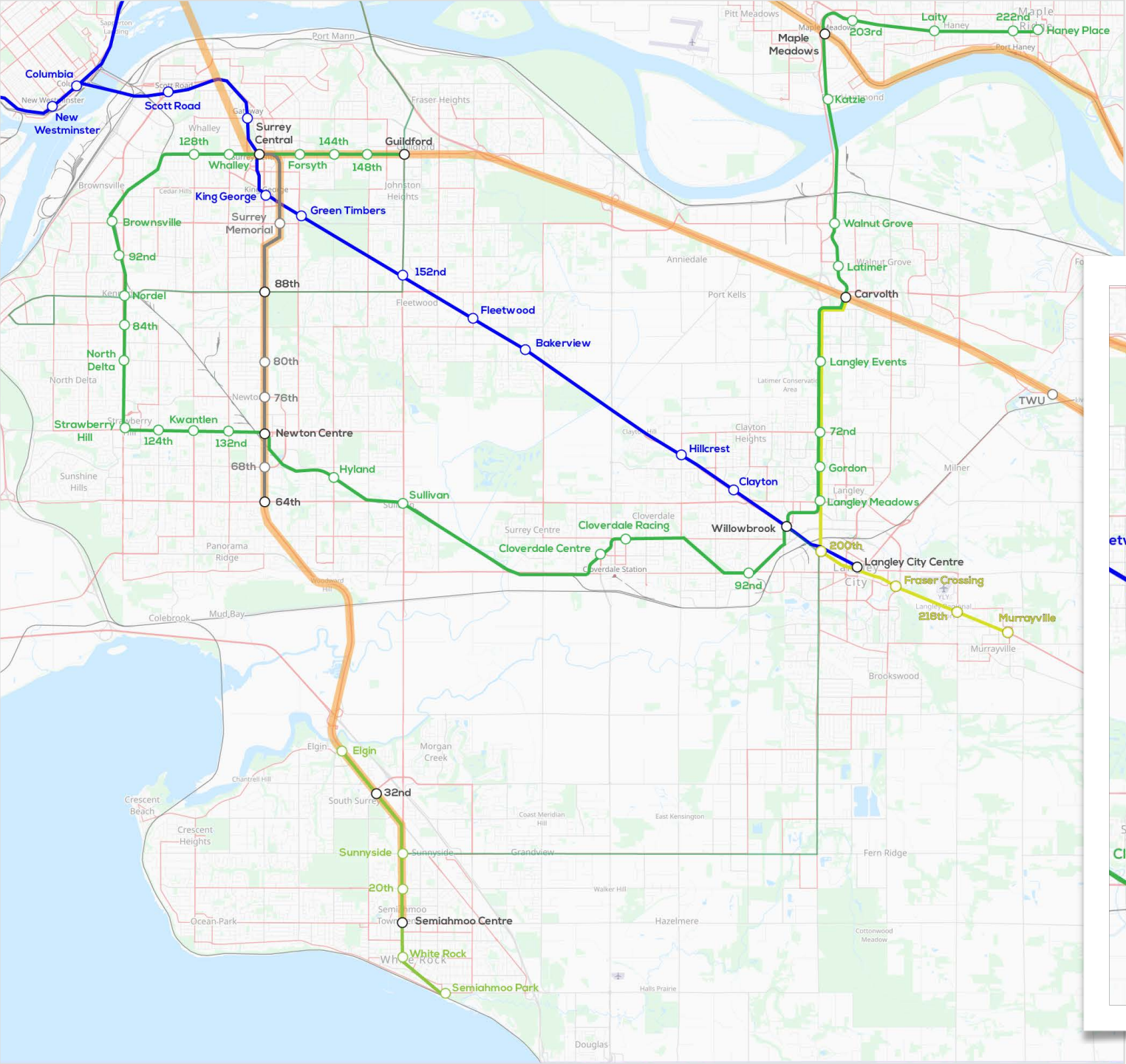
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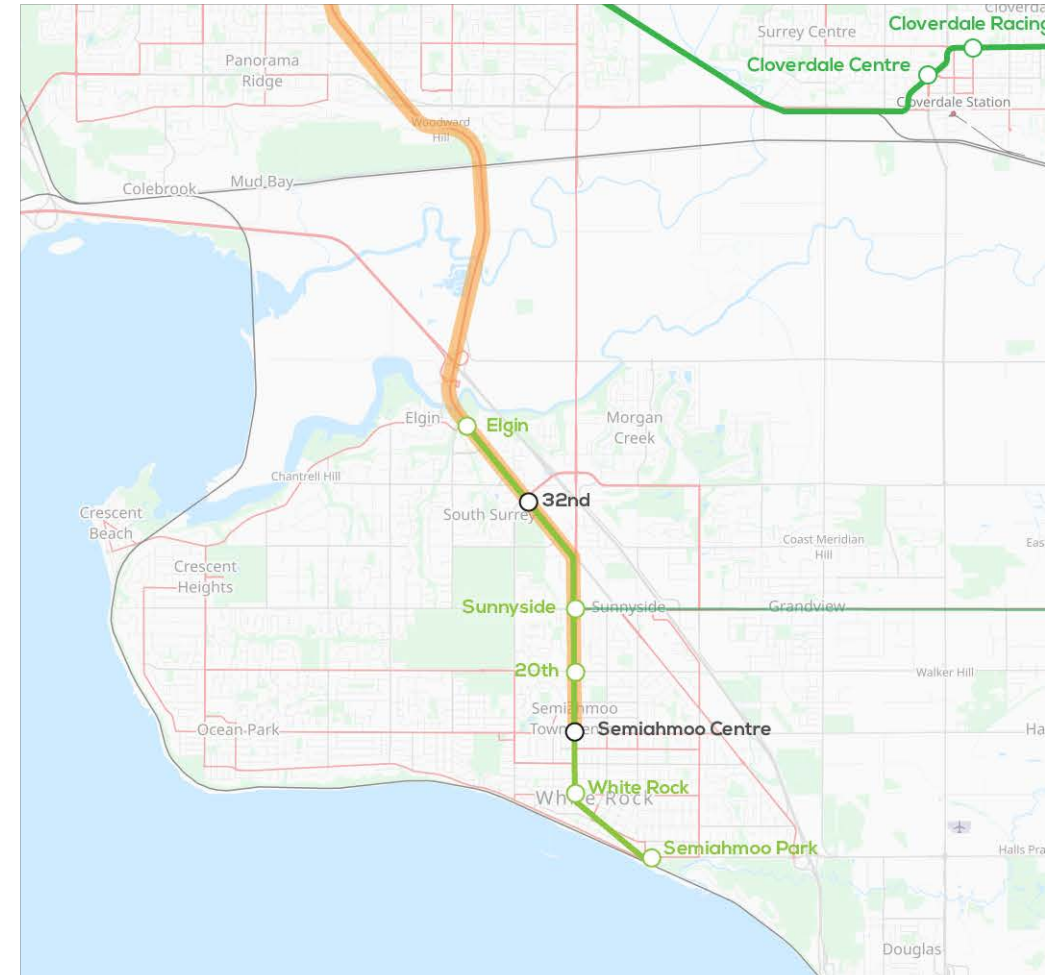
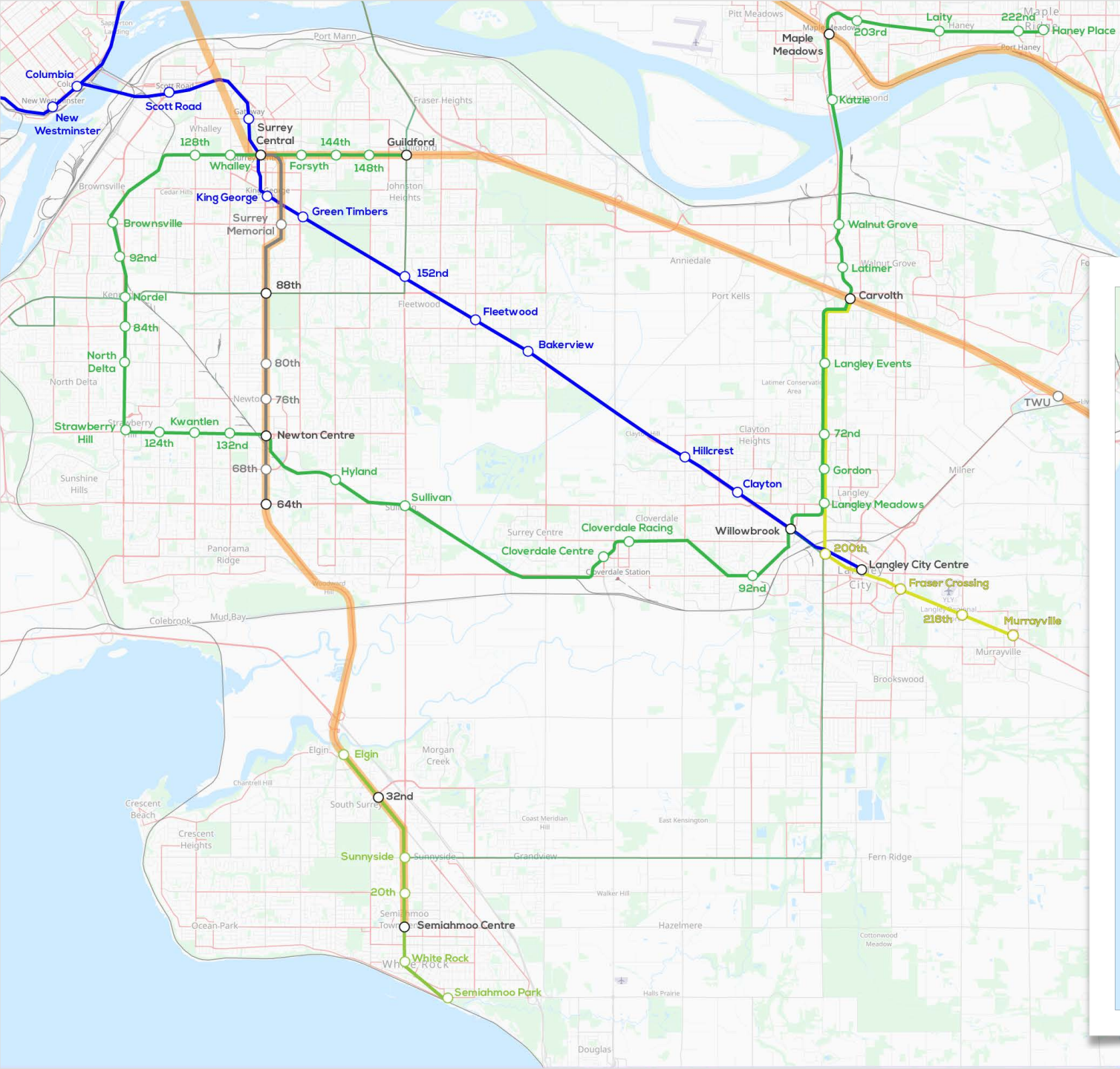
Surrey Rise



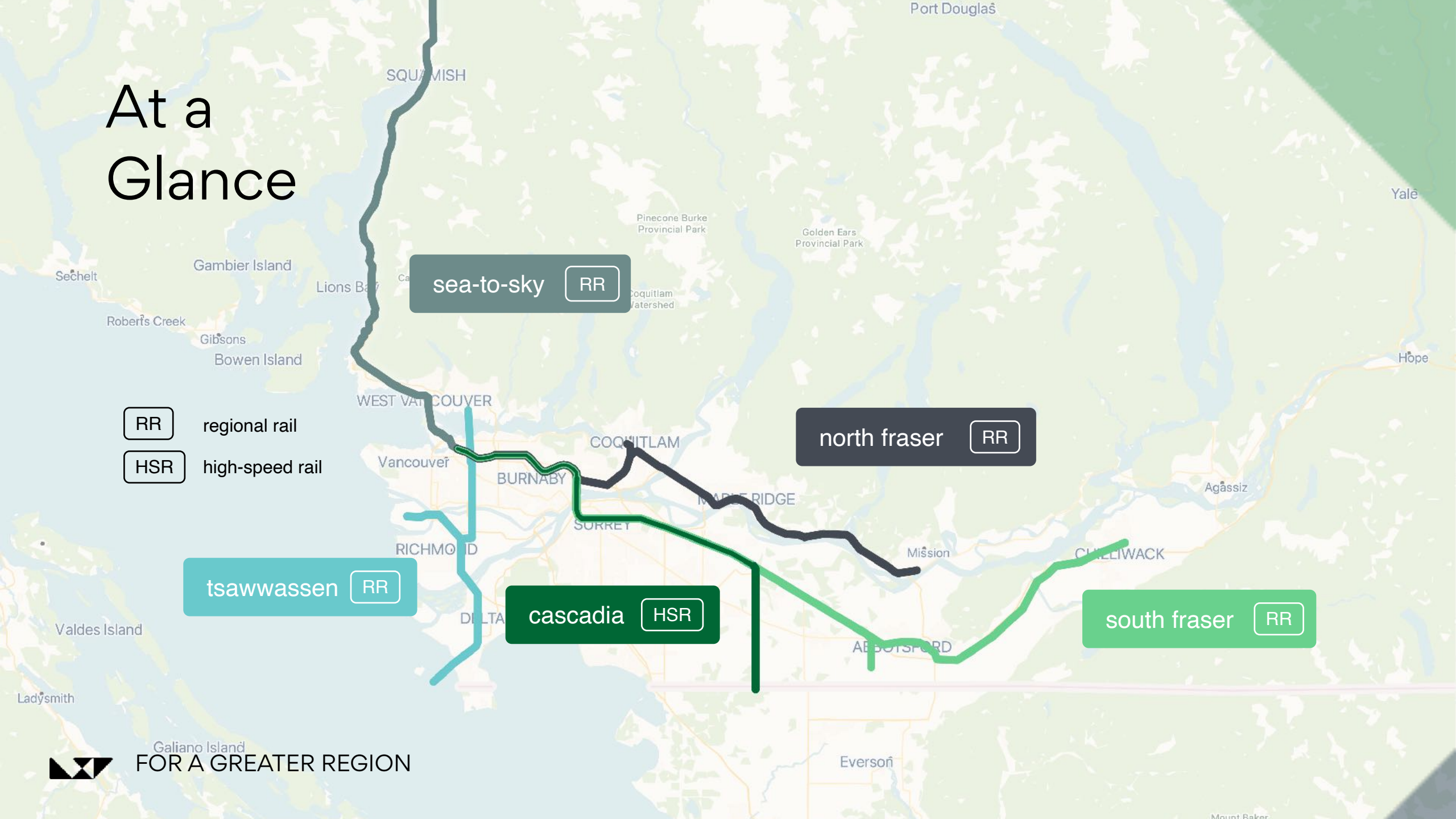
Langley Rise



Semiahmoo



At a Glance



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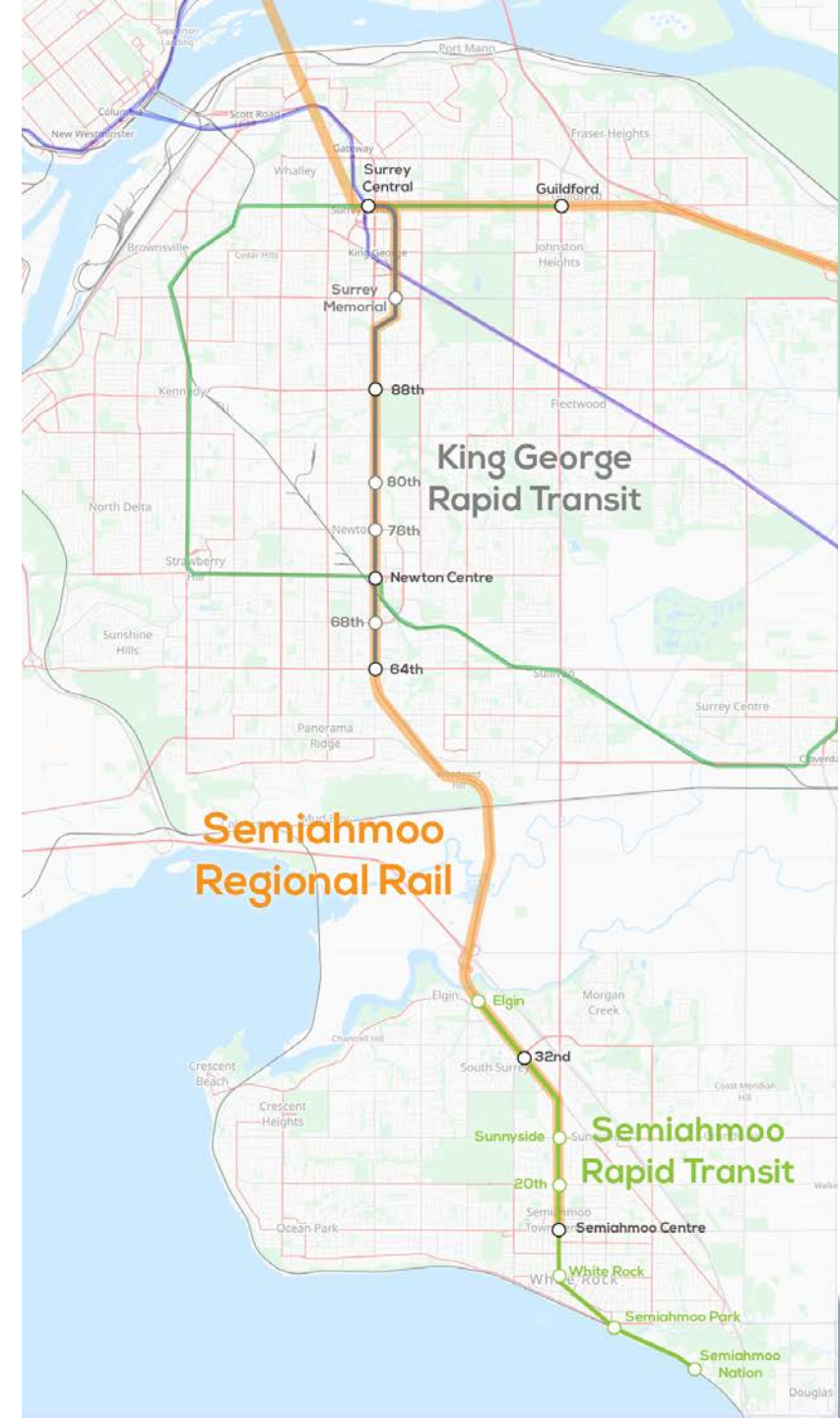
Nexus Phase 1





King George Skytrain

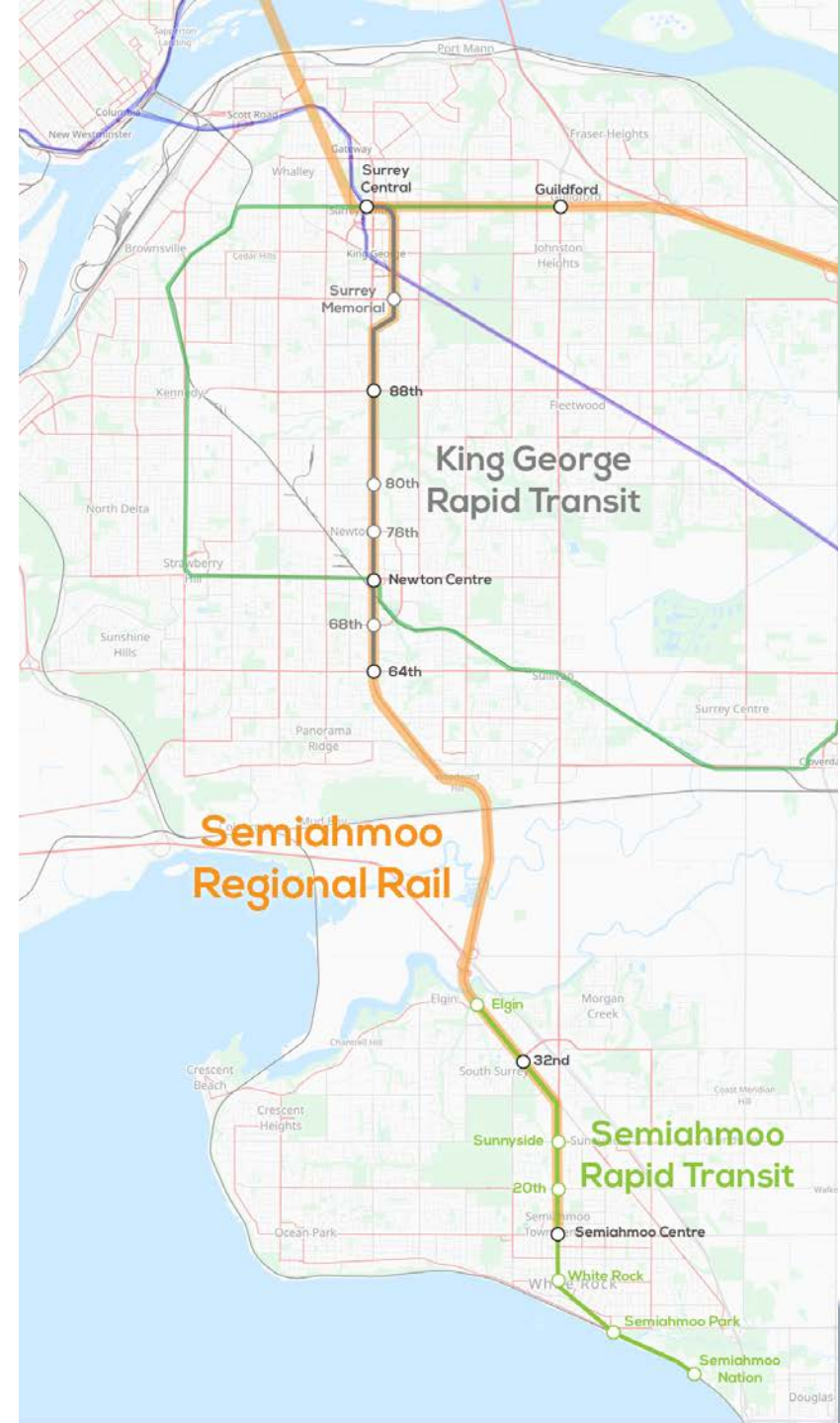
Cost: \$1.77 B
SM to SC: 27 min



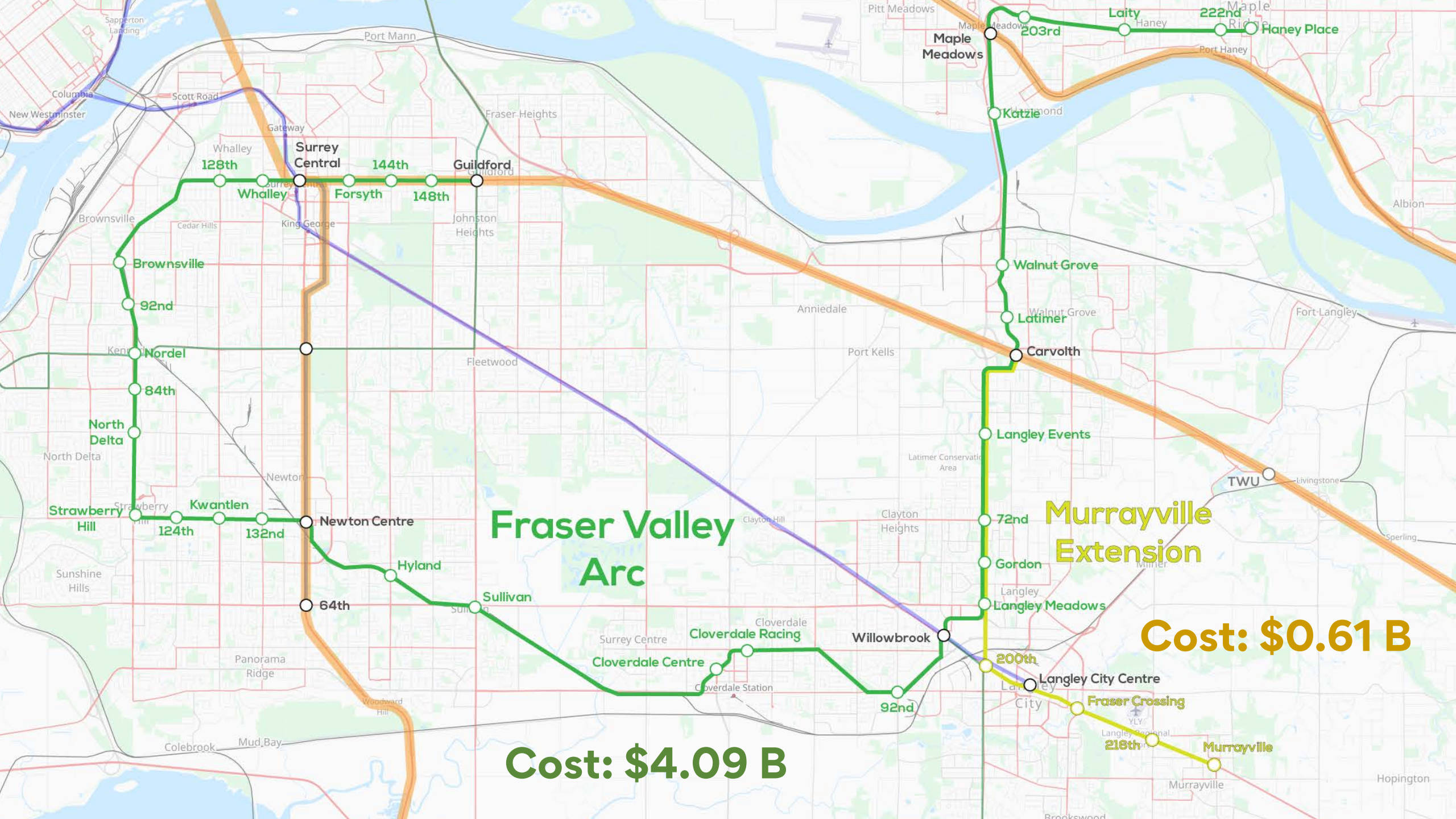
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Semiahmoo RR+RT

Cost: \$2.21 B
SM to SC: 17 min



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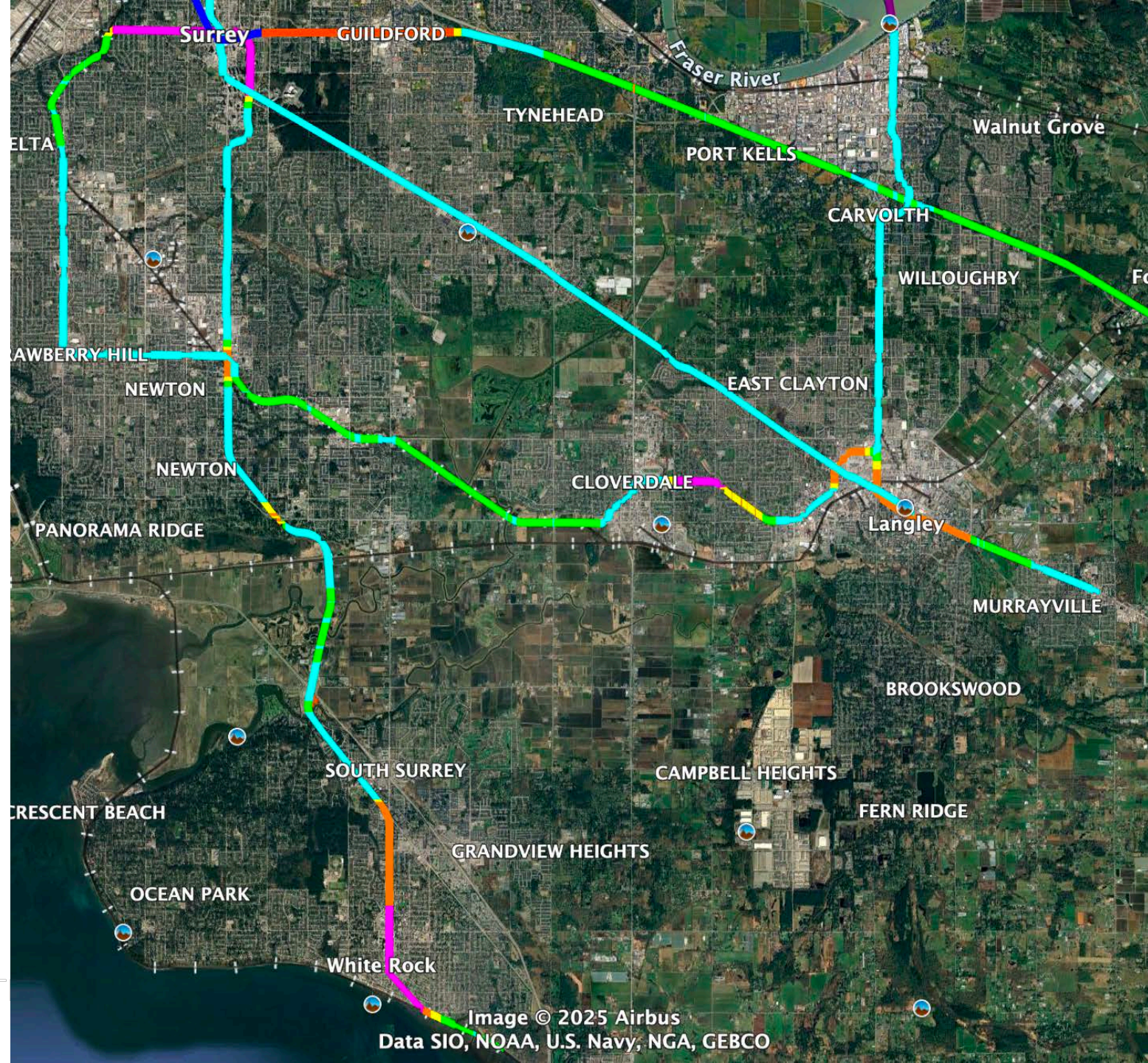


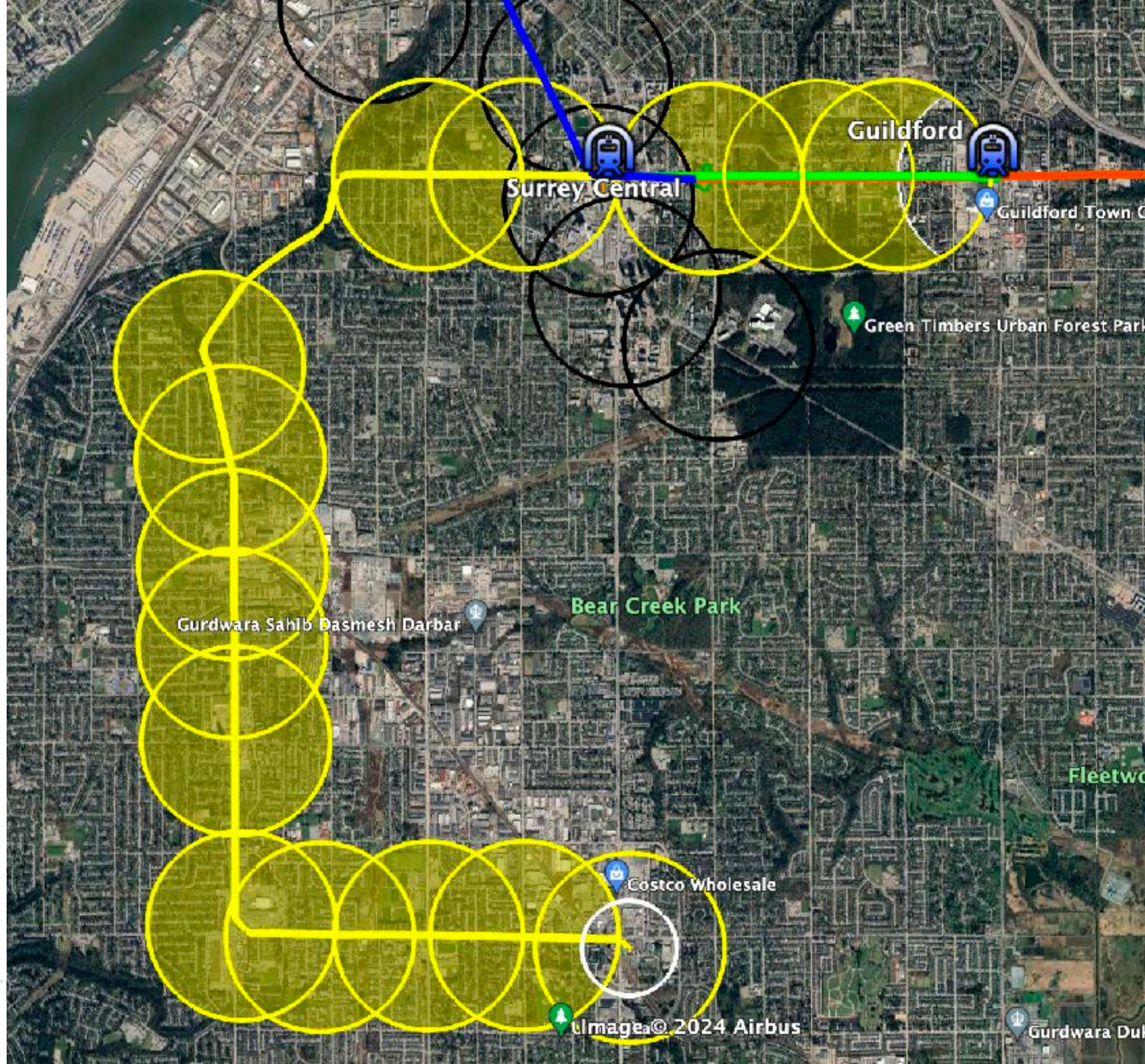
Fraser Valley Arc

Cost: \$4.09 B

Murrayville Extension

Cost: \$0.61 B





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Summary

Line	Length	# Stations	Cost (Unshared)	Cost (Shared)
Semiahmoo RR + RT	20.2 km	15	\$2.21 B	\$1.50 B
Fraser Valley Arc	47.8 km	33	\$4.09 B	\$3.53 B
Murrayville Extension	5.90 km	5	\$0.61 B	\$0.61 B
TOTAL	73.9 km	53	\$6.91 B	\$5.61 B



Transit vs. Car Travel Times: Now

	SC	GF	NT	FW	ND	SM	CT	CD	LC	CV	WB	MV	BW	AG
Surrey Central	X	56%	86%	84%	76%	70%	86%	53%	69%	65%	42%	69%	67%	45%
Guildford	56%	X	63%	75%	60%	74%	52%	59%	48%	54%	37%	42%	47%	29%
Newton	86%	63%	X	67%	80%	65%	45%	45%	52%	44%	33%	58%	51%	47%
Fleetwood	84%	75%	67%	X	49%	57%	64%	55%	58%	35%	27%	53%	52%	50%
North Delta	76%	60%	80%	49%	X	41%	53%	40%	61%	56%	51%	51%	43%	46%
Semiahmoo	70%	74%	65%	57%	41%	X	38%	52%	58%	45%	46%	44%	75%	39%
Clayton	86%	52%	45%	64%	53%	38%	X	28%	82%	26%	24%	77%	24%	71%
Cloverdale	53%	59%	45%	55%	40%	52%	28%	X	70%	29%	30%	63%	41%	56%
Langley Centre	69%	48%	52%	58%	61%	58%	82%	70%	X	57%	36%	75%	63%	80%
Carvolth	65%	54%	44%	35%	56%	45%	26%	29%	57%	X	56%	40%	47%	38%
Willoughby	42%	37%	33%	27%	51%	46%	24%	30%	36%	56%	X	33%	31%	35%
Murrayville	69%	42%	58%	53%	51%	75%	77%	63%	75%	40%	33%	X	57%	59%
Brookwood	67%	47%	51%	52%	43%	75%	24%	41%	63%	47%	31%	57%	X	39%
Aldergrove	45%	29%	47%	50%	46%	39%	71%	56%	80%	38%	35%	59%	39%	X



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Transit vs. Car Travel Times: Future

	SC	GF	NT	FW	ND	SM	CT	CD	LC	CV	WB	MV	BW	AG
Surrey Central	X	300%	257%	160%	183%	235%	188%	124%	167%	300%	147%	121%	102%	67%
Guildford	300%	X	133%	75%	144%	140%	92%	74%	114%	233%	108%	89%	80%	52%
Newton	257%	133%	X	73%	200%	220%	109%	200%	118%	150%	100%	129%	81%	74%
Fleetwood	160%	75%	73%	X	81%	81%	175%	55%	138%	70%	55%	113%	72%	65%
North Delta	183%	144%	200%	81%	X	104%	106%	153%	150%	160%	133%	105%	78%	82%
Semiahmoo	235%	140%	220%	81%	104%	X	74%	92%	70%	133%	95%	76%	75%	51%
Clayton	188%	92%	109%	175%	106%	74%	X	77%	350%	88%	71%	143%	37%	130%
Cloverdale	124%	74%	200%	55%	153%	92%	77%	X	123%	113%	100%	114%	47%	80%
Langley Centre	167%	114%	118%	138%	150%	70%	350%	123%	X	160%	113%	240%	67%	86%
Carvolth	300%	233%	150%	70%	160%	133%	88%	113%	160%	X	250%	113%	73%	67%
Willougby	147%	108%	100%	55%	133%	95%	71%	100%	113%	250%	X	114%	65%	59%
Murrayville	121%	89%	129%	113%	105%	75%	143%	114%	240%	113%	114%	X	43%	89%
Brookwood	102%	80%	81%	72%	78%	75%	37%	47%	67%	73%	65%	43%	X	51%
Aldergrove	67%	52%	74%	65%	82%	51%	130%	80%	86%	67%	59%	89%	51%	X



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Key Recommendations

Transit Cost Control is Essential

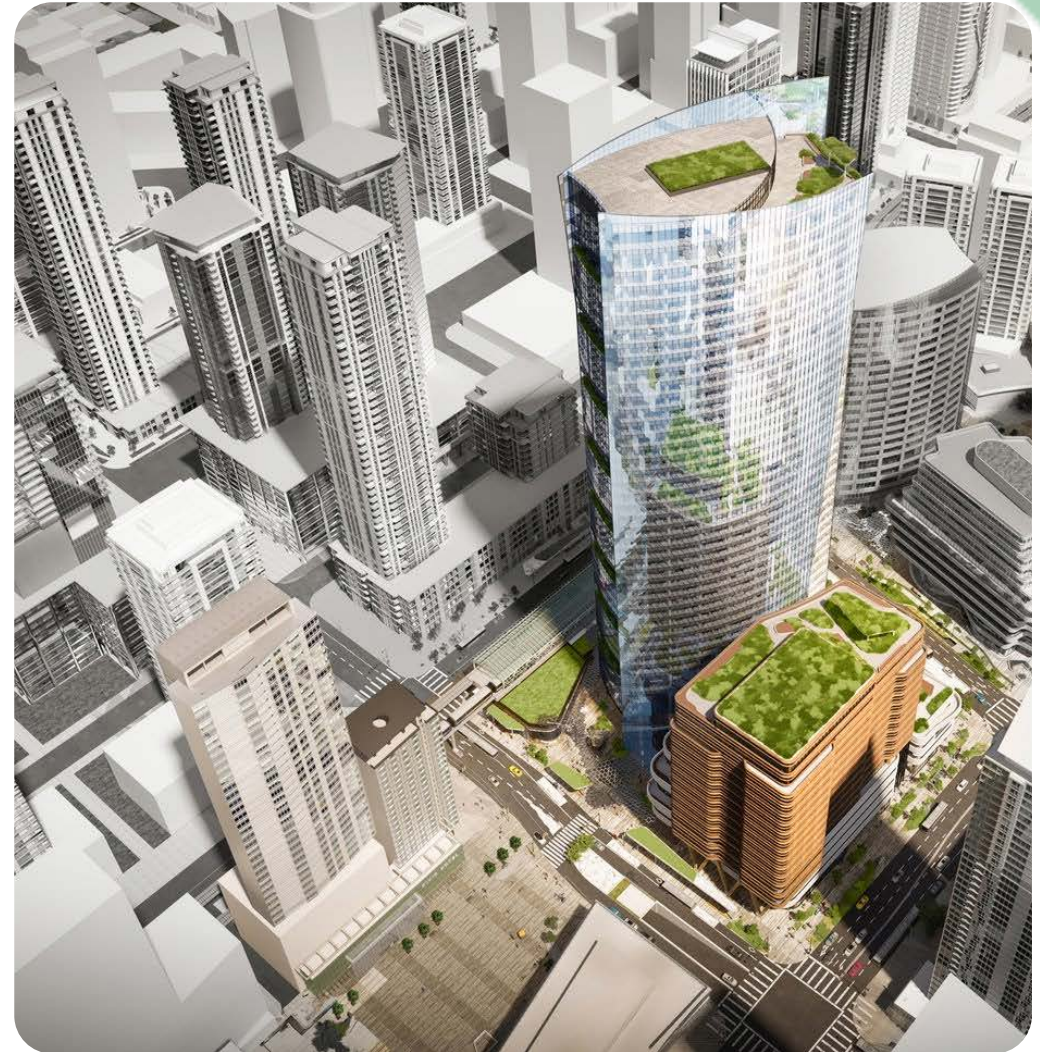
- Need in-house expertise
- Smaller cities have extensive rapid

Protect Future Corridors/Station Locations

- Will greatly reduce construction costs
- Requires municipal cooperation

Use Development to Help Finance It

- Development Right around future stations
- Offset need for funds from higher-levels of government



Thank you



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